



Madison, Wisconsin

CITY OF MADISON

CITY ENGINEERING DIVISION

DEPARTMENT OF PUBLIC WORKS

PLAN OF PROPOSED IMPROVEMENT

PUBLIC IMPROVEMENT PROJECT APPROVED

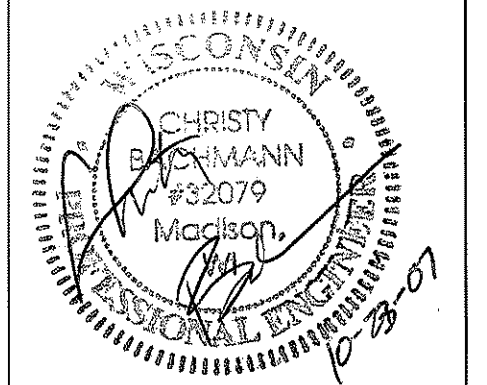
OCTOBER 16, 2007

BY THE COMMON COUNCIL
OF MADISON, WISCONSIN

PUBLIC IMPROVEMENT DESIGN
APPROVED BY:

Christy Bachmann 10/16/07
City Engineer Date

STREET
DESIGNED BY:



INDEX OF SHEETS

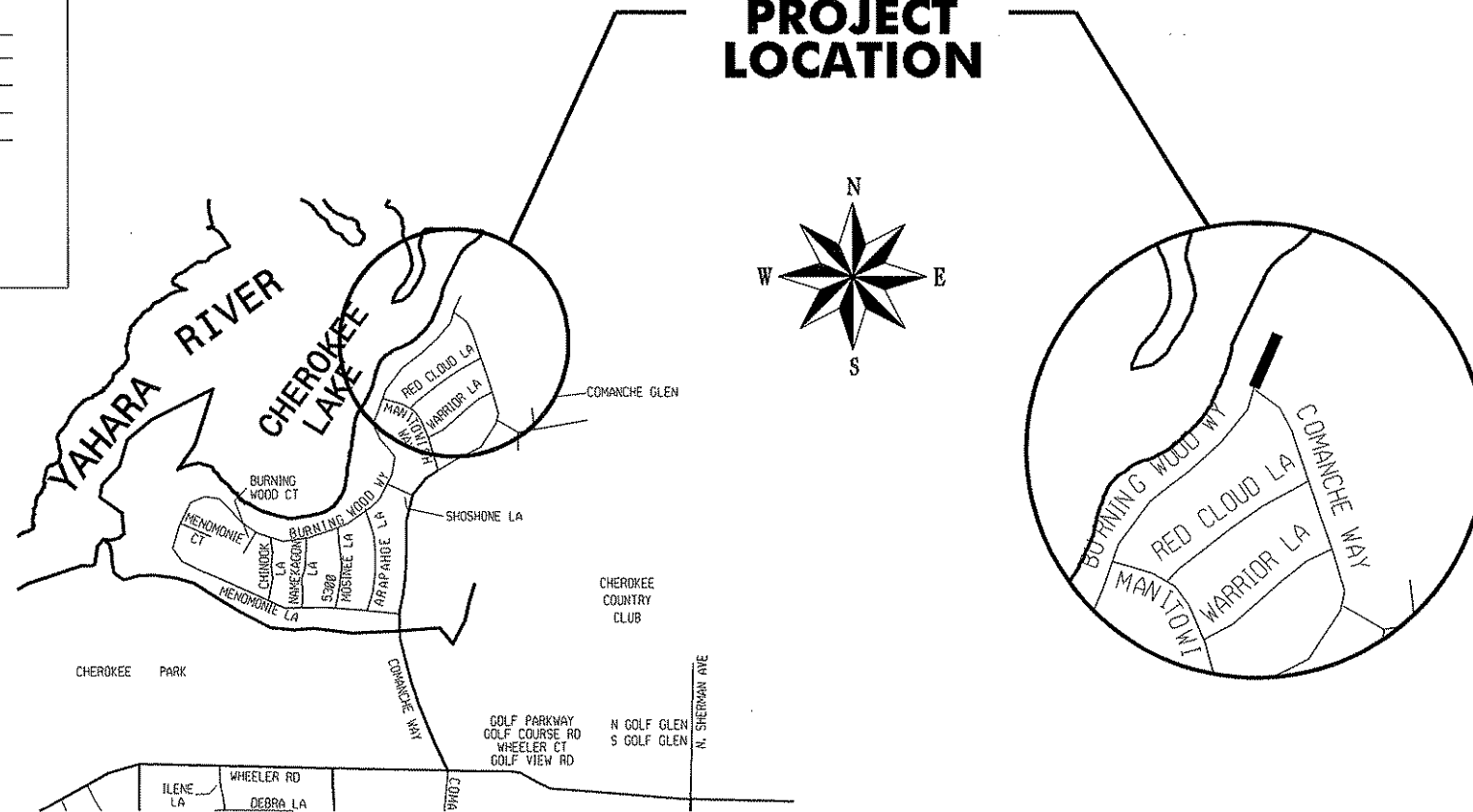
SHEET NO.	TITLE
1	DETAILS
D1-D3	BURNING WOOD WAY PLAN AND PROFILE
P1	SIDEWALK PLAN AND PROFILE
P2	BIKE PATH PLAN AND PROFILE
P3	BURNING WOOD WAY STORM SEWER
U1	BURNING WOOD WAY SANITARY SEWER
U2	STORM SEWER SCHEDULE
U3	BURNING WOOD WAY & BIKE PATH WATER

1507 BURNING WOOD WAY CSM

CITY PROJECT NO. 53B2177
CONTRACT NO. 2177

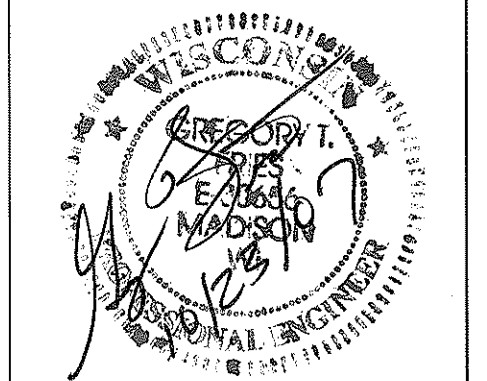
CONVENTIONAL SIGNS	
FIELD VERIFY ALL UTILITY LOCATIONS	
GAS	— G —
STORM SEWER	— ST —
SANITARY SEWER	— SAN —
WATER	— W —
OVERHEAD ELECTRIC	— OH —
POWER POLE	⊥
ADA COMPLIANT RAMP W/ DETECTABLE WARNING FIELD	□
COMBUSTIBLE FLUIDS	

PROJECT LOCATION



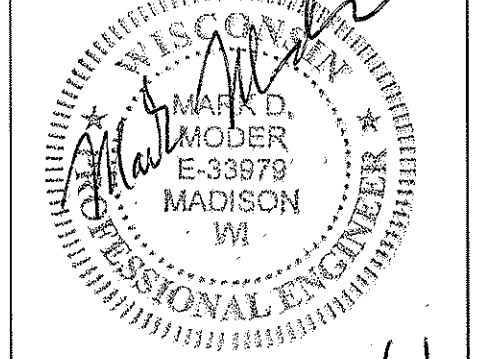
GEOMETRICS
DESIGNED BY:

STORM SEWER
DESIGNED BY:



WATER
DESIGNED BY:

SANITARY SEWER
DESIGNED BY:



THE LOCATION AND INFORMATION FOR PROPOSED NEW TREES, IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS ARE APPROXIMATE AND ARE SHOWN FOR REFERENCE ONLY. THE LOCATIONS, SPECIFICATIONS AND PLANTING METHODS OF ALL PROPOSED NEW OR REPLACEMENT TREES IN THE PUBLIC RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE APPROVED BY THE CITY FORESTER PRIOR TO INSTALLATION.

NO TREES IN THE RIGHT OF WAY OR ON PUBLIC LANDS SHALL BE TRIMMED, PRUNED, REMOVED OR ADVERSELY AFFECTED IN ANY WAY UNTIL THE DEVELOPER HAS RECEIVED WRITTEN PERMISSION FROM THE CITY ENGINEER OR CITY FORESTER. SAID WRITTEN PERMISSION SHALL INCLUDE LANGUAGE INDICATING THAT SECTION 10.101 OF THE MADISON GENERAL ORDINANCES AND ADMINISTRATIVE PROCEDURE MEMORANDUM NO. 6-2, REFERING TO NOTIFICATION OF PROPERTY OCCUPANTS AND/OR OWNERS, HAS BEEN COMPLIED WITH.

ALL PAVEMENT IN THE BURNING WOOD WAY AND COMANCHE WAY RIGHT OF WAY AND ALL PAVEMENT FOR THE BIKE PATH SHALL BE TYPE "A", AND ALL PAVEMENT FOR TRENCH PATCHES SHALL BE TYPE "C".

ALL ISLANDS SHALL HAVE TYPE "E" C & G PER S.D.D. 3.08 (EXCEPT WHERE SPECIFIED DIFFERENTLY ON DRAWING) AND MOUNTABLE NOSES PER S.D.D. 3.13. ALL OTHER C & G SHALL BE TYPE "A" PER S.D.D. 3.06 UNLESS OTHERWISE NOTED.

ISLAND INTERIORS SHALL BE OF SOD WHERE THE BACK TO BACK OF CURB DIMENSION IS GREATER THAN 6'.

UNDERDRAINS SHALL BE INSTALLED, PER STANDARD DETAIL DRAWING 4.05 FOR 75' ON EACH SIDE OF THE LOW POINT, OR TO THE NEAREST CURB HIGH POINT. ALL UNDERDRAIN SHALL BE WRAPPED.

ALL GUTTERS SHALL DRAIN WITH A MINIMUM GRADES OF 0.5% TOWARD STORM SEWER INLETS.

PAVEMENT CROSS SLOPES SHALL BE 2%, TERRACES SHALL SLOPE AT A 4% GRADE TOWARD THE GUTTER, SHOULDERS SHALL HAVE A 4% CROSS SLOPE AND CROSS SLOPES FORMING DITCHES SHALL NOT EXCEED 4.00:1 UNLESS OTHERWISE INDICATED.

THE CROSS SLOPE OF SIDEWALKS AND BARRIER FREE SIDEWALK CURB RAMPS SHALL BE 2%. THE LONGITUDINAL GRADE OF BARRIER FREE SIDEWALK CURB RAMPS SHALL NOT EXCEED 8.33%. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED ACCORDING TO S.D.D. 3.03. AT ALL OTHER LOCATIONS THE LONGITUDINAL GRADE OF SIDEWALKS SHALL NOT EXCEED 5.0 % OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER NOR BE LESS THAN 0.5% AND SHALL DRAIN TOWARD STORM SEWER INLETS. A 2 FOOT WIDE CONCRETE FLUME SHALL BE INSTALLED BETWEEN THE SIDEWALK AND THE BACK OF CURB AT SIDEWALK LOW POINTS WHICH CANNOT OTHERWISE BE DRAINED DIRECTLY TO THE GUTTER. SIDE SLOPES WITHIN TEN FEET OF A PUBLIC SIDEWALK SHALL NOT EXCEED 4.00:1. THE DESIGN OF SIDEWALK ELEVATIONS AND GRADES IS INCLUDED IN THIS PLAN, BUT ALL SIDEWALK AND SIDEWALK RAMP ELEVATIONS AND GRADES SHALL BE FIELD VERIFIED AND SET TO COMPLY WITH THE CITY OF MADISON STANDARD SPECIFICATIONS AND THE A.D.A. GUIDELINES.

OBTAIN A PRINT OUT OF THE ALIGNMENT FROM THE CITY ENGINEER PRIOR TO STAKING THIS PROJECT.

CURB STATION AND OFFSETS SHALL BE TO THE FACE OF CURB UNLESS OTHERWISE INDICATED. CURB ELEVATIONS SHALL BE TO THE TOP OF CURB (OR EXTENDED TOP OF CURB FOR DRIVEWAYS OR RAMPS) UNLESS OTHERWISE INDICATED.

POWER POLES AND OTHER OBSTRUCTIONS SHALL BE MOVED TO PROVIDE 2 FEET MINIMUM OF CLEAR DISTANCE FROM ANY FACE OF CURB OR EDGE OF SIDEWALK.

ANY INFORMATION SHOWN ON THIS PLAN, WHICH IS NOT PART OF THIS PROJECT, IS PRELIMINARY AND NOT FOR CONSTRUCTION.

THERE MAY BE EXISTING UTILITIES OR OTHER FEATURES WHICH ARE EITHER NOT SHOWN OR SHOWN INCORRECTLY ON THIS PLAN. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO LOCATE AND IDENTIFY ALL UTILITIES AND TOPOGRAPHY WHICH MAY AFFECT THE CONSTRUCTION OF THESE IMPROVEMENTS.

ALL PERMANENT SIGNING AND POSTING WILL BE DETERMINED AND PROVIDED BY THE TRAFFIC ENGINEERING DIVISION, FOLLOWING CONSTRUCTION OF THESE IMPROVEMENTS.

THE DEVELOPER SHALL PROVIDE, INSTALL AND MAINTAIN ALL STREET END BARRICADES, SIGNING AND TRAFFIC CONTROL, AS REQUIRED BY THE CITY TRAFFIC ENGINEER.

PAVEMENT SAWCUTS SHALL BE AS DIRECTED BY THE CITY CONSTRUCTION ENGINEER. SAWCUTS SHOWN ON THE PLAN ARE APPROXIMATE.

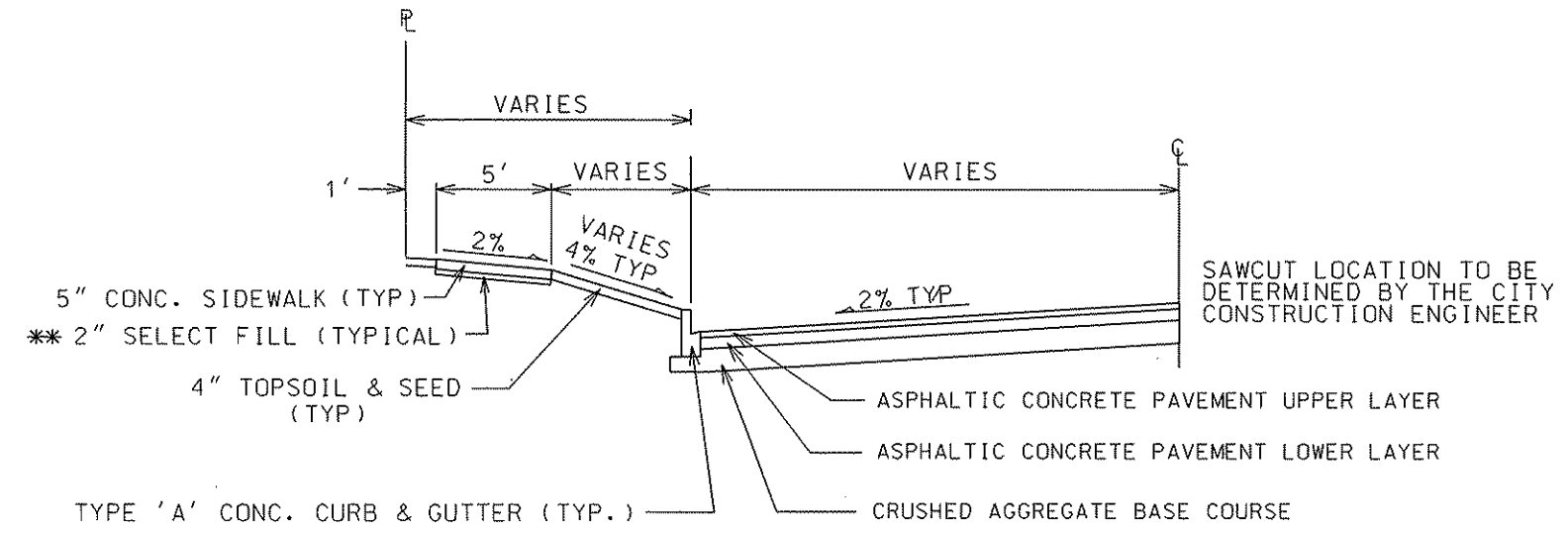
CURB ON CUL DE SACS SHALL BE INSTALLED ACCORDING TO SDD 3.05.

PLOT SCALE:

PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



TYPICAL TERRACE SECTION
BURNING WOOD WAY & COMANCHE WAY
TAKEN FROM SDD 4.02 & 4.05
NOT TO SCALE

Pavement Patching Criteria

**** Criteria use on:
Streets with pavement rating > 6
Arterial Streets

1. Length of Patch

- a. Minimum 50 feet long
- b. Minimum of 15 feet beyond the excavation
- c. Where multiple patches are created and the separation between them is less than 100 feet, the patches shall be combined into a single patch.
- d. The patches shall be adjusted in the field to meet special conditions such as previous paving or patching limits.

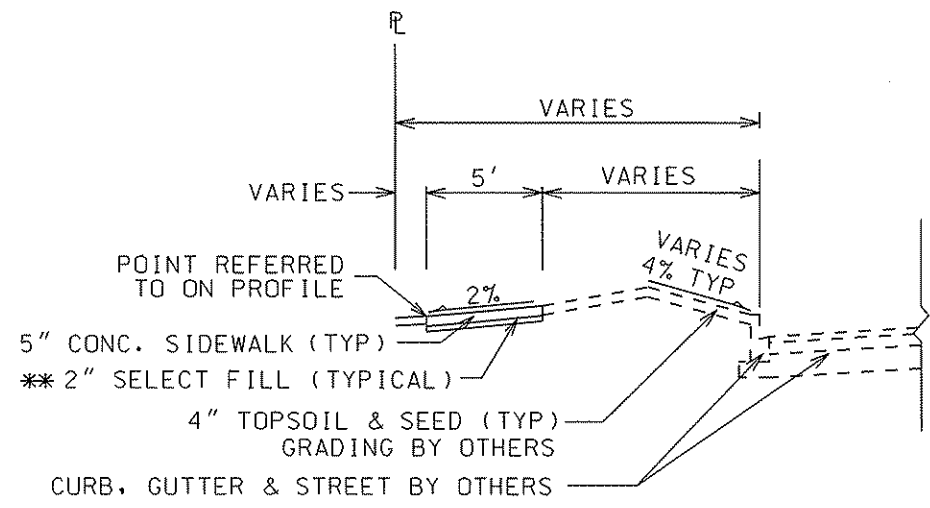
2. Width of Patch (all dimensions are curb face to curb face)

a. All Streets Except Divided or one way roadways

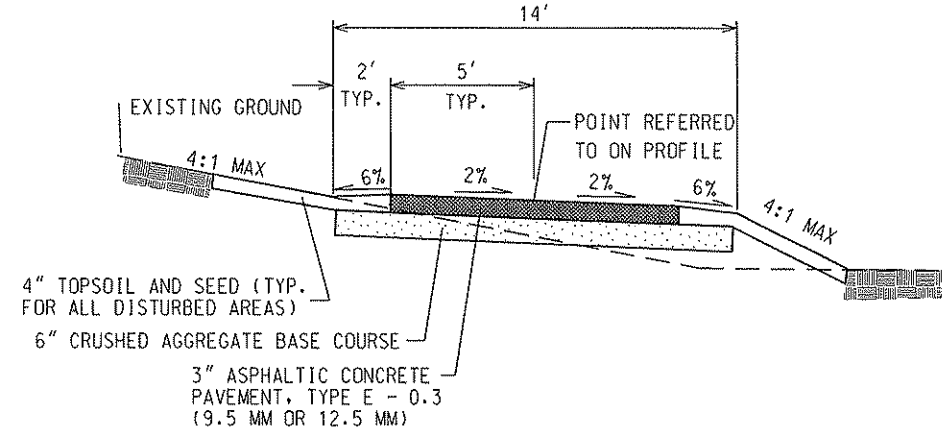
- i. Street Width 0 to 24 feet wide - Patch entire street width
- ii. Street Width 25 to 37 feet wide - Patch one half the street width (curb to centerline of roadway). Note - Utility Engineer may adjust paving limit to correspond with a painted centerline in situations where the painted centerline is not in the center of the street.
- iii. Street Width 38 feet and up - Patch width of entire lane for each lane which was disturbed by the excavation.
 - 1. If the lane is adjacent to a bike lane, include the bike lane. (except when there is a parking lane between the bike lane and the curb)
 - 2. If the lane is a bike lane and adjacent to a parking lane, include the parking lane.
 - 3. If the lane is a bike lane and not adjacent to a parking lane, include the adjacent travel lane.

b. Divided Roadways and One Way Streets

- i. Street 0 to 19 feet wide - Patch entire street width
- ii. Street Width 20 feet and up - Patch width of entire lane for each lane which was disturbed by the excavation.
 - 1. If the lane is adjacent to a bike lane, include the bike lane. (except when there is a parking lane between the bike lane and the curb)
 - 2. If the lane is a bike lane and adjacent to a parking lane, include the parking lane.
 - 3. If the lane is a bike lane and not adjacent to a parking lane, include the adjacent travel lane.



TYPICAL TERRACE SECTION
NEW SIDEWALK
TAKEN FROM SDD 4.02 & 4.05
NOT TO SCALE



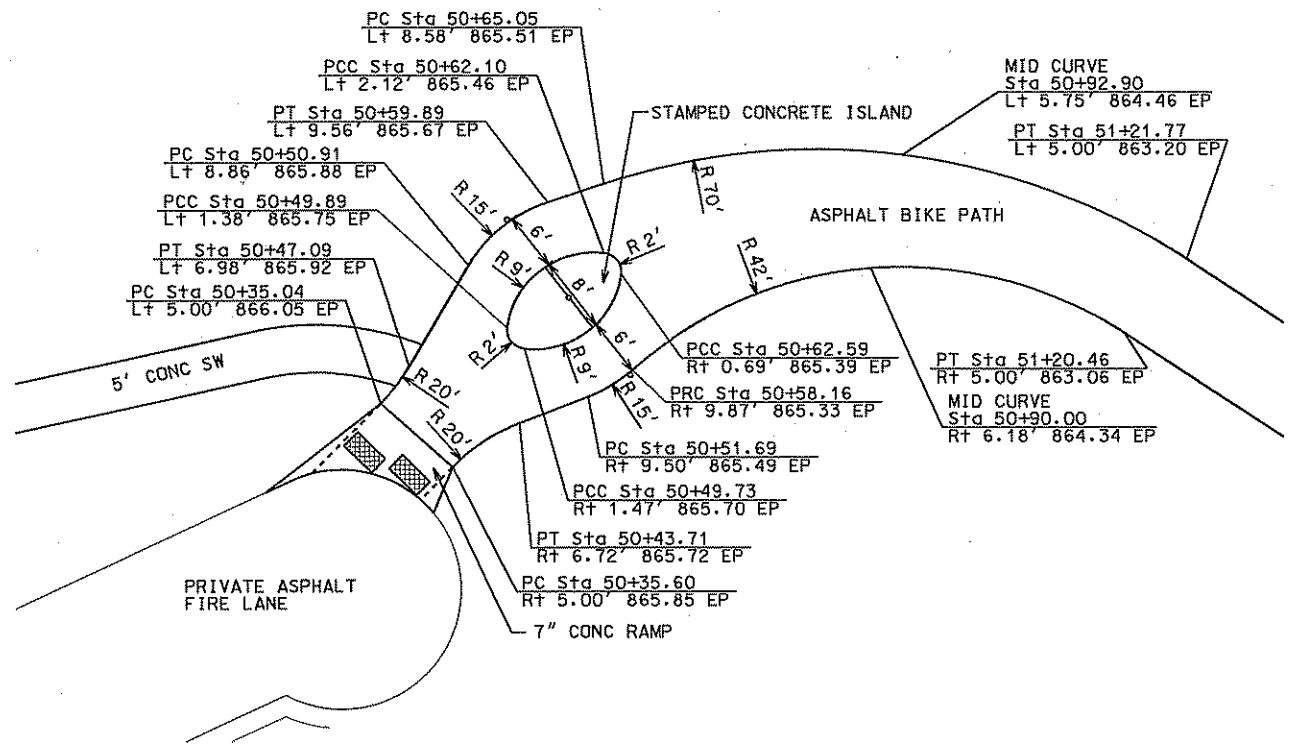
TYPICAL BIKE PATH SECTION
TAKEN FROM SDD 4.08
NOT TO SCALE

NOTES:

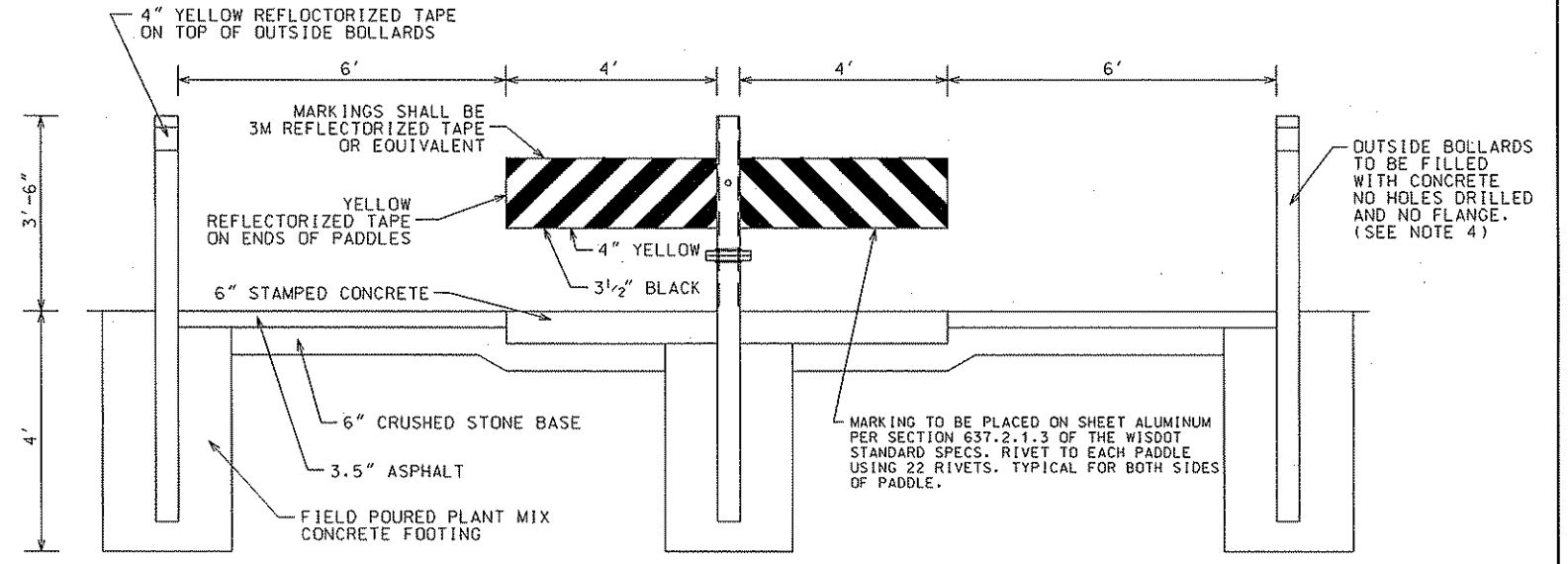
- * A PAVEMENT DESIGN IN ACCORDANCE WITH STANDARD DETAIL DRAWING 4.06 IS REQUIRED ON ALL PROJECTS
- **3" OF CRUSHED AGGREGATE BASE COURSE GRADATION 3 SHALL BE PLACED UNDER THE SIDEWALK WHERE THE CENTERLINE GRADE OF THE STREET EXCEEDS 5%
- *** ALL UPPER LAYER PAVEMENTS ARE 12.5 mm; LOWER LAYER PAVEMENT IS 12.5 mm FOR TYPE A & 19 mm FOR TYPE B & C.
- **** PAVEMENT RATING ON BURNING WOOD WAY = 6
PAVEMENT RATING ON COMANCHE WAY = 6

*CITY OF MADISON MINIMUM PAVEMENT DESIGN

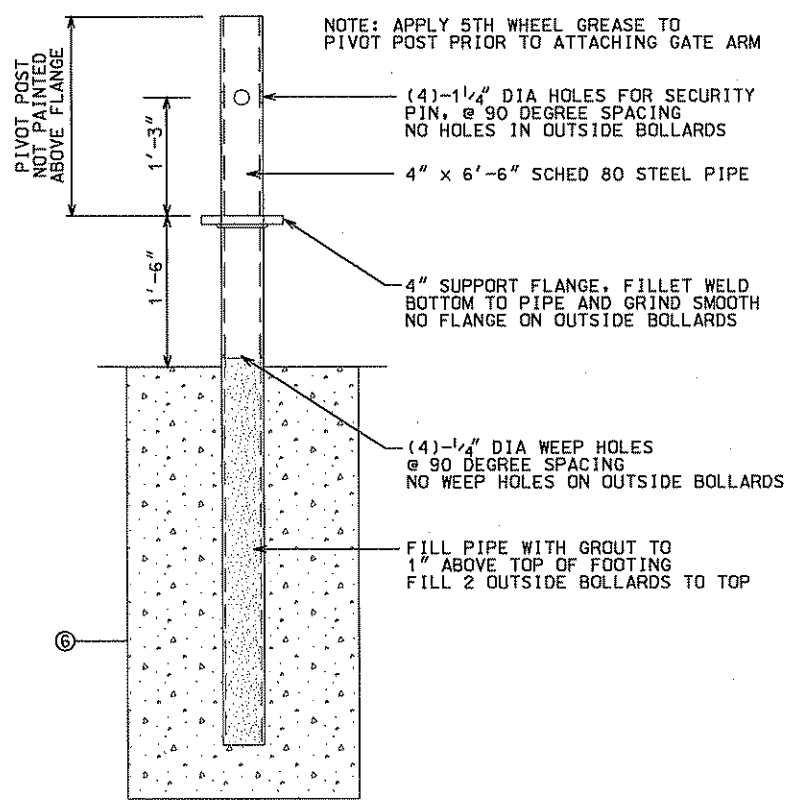
TYPE	CRUSHED AGGREGATE BASE COURSE		ASPHALTIC CONCRETE PAVEMENT			
	LOWER LAYER GRADATION 1	UPPER LAYER GRADATION 2	LOWER LAYER		UPPER LAYER	
			TYPE	THICKNESS	TYPE	THICKNESS
A	6"	4"	E-0.3	1.75"	E-0.3	1.75"
B	6"	4"	E-1	2.50"	E-1	1.75"
C	6"	4"	E-3	3.50"	E-3	1.75"



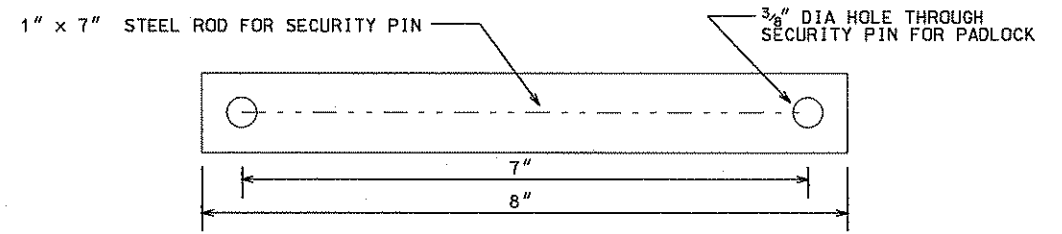
PLAN
SCALE 1" = 20'



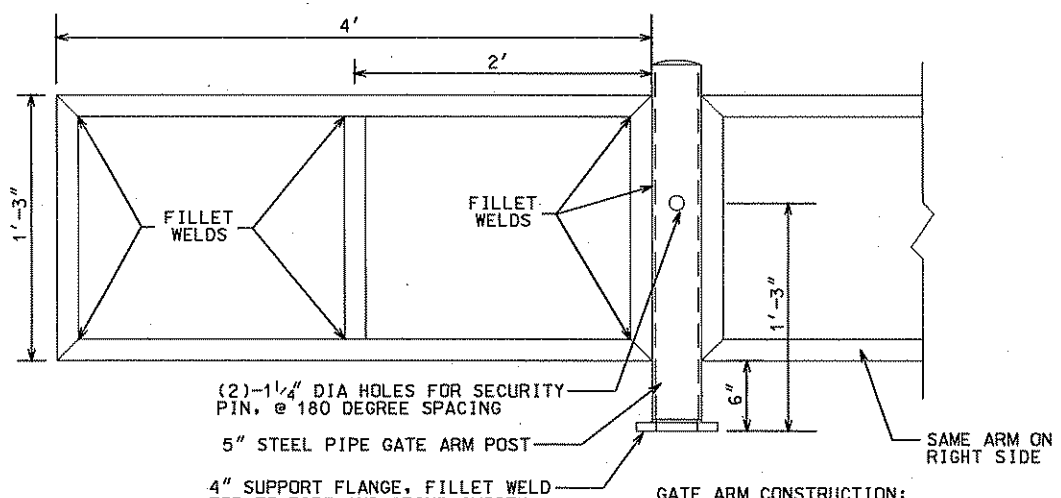
PROFILE
NTS



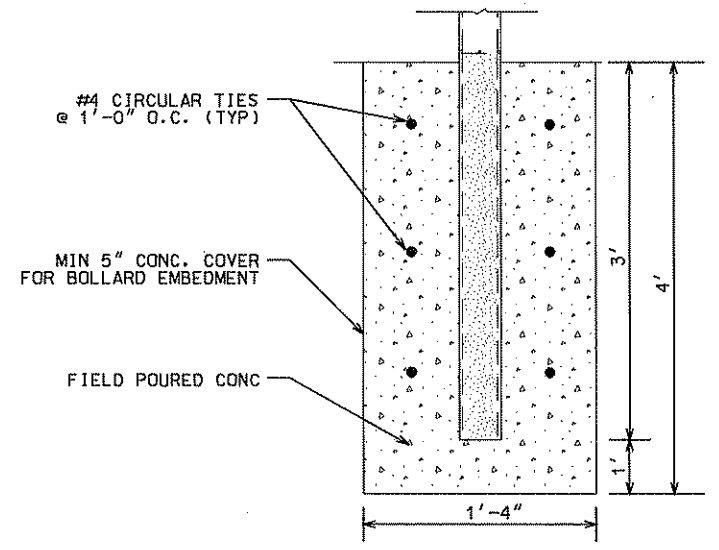
GATE PIVOT POST DETAIL



SECURITY PIN DETAIL



GATE ARM DETAIL



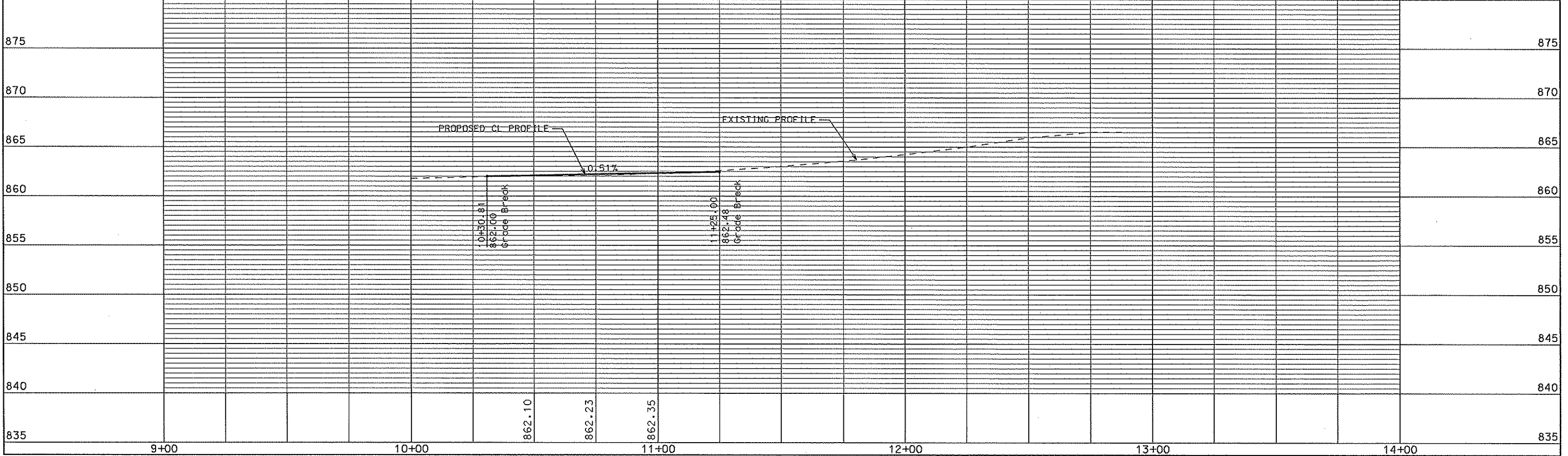
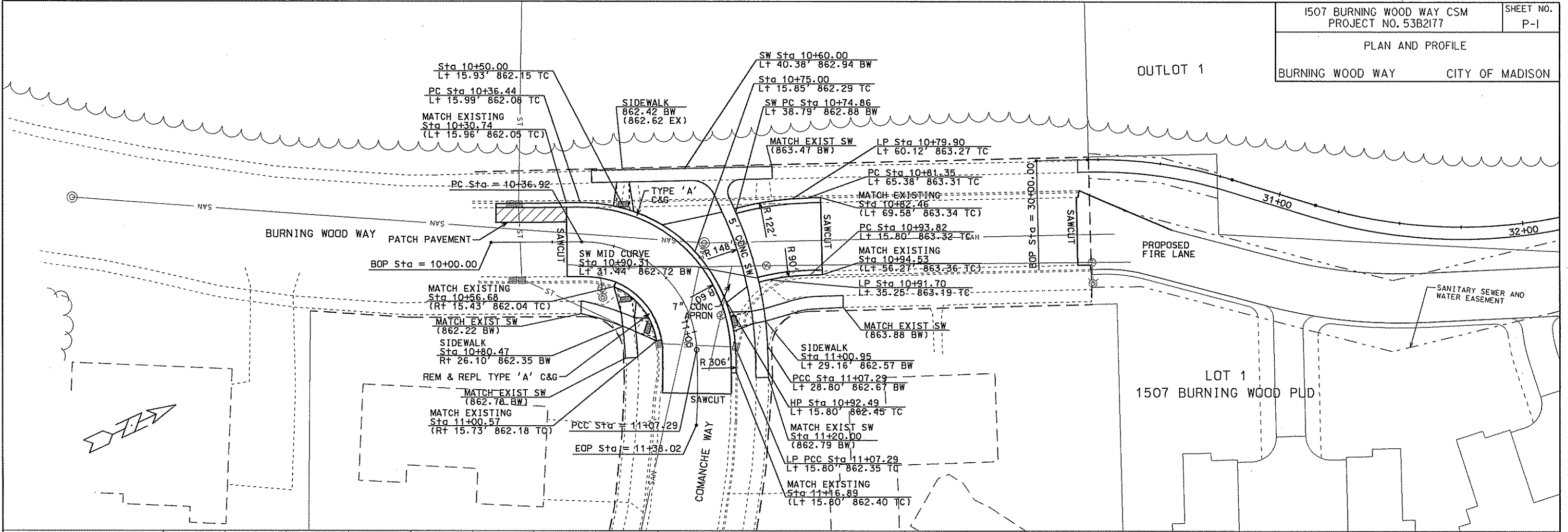
FOOTING DETAIL

- SPECIFICATIONS AND NOTES:
1. ALL STEEL PIPE TO BE SCHEDULE 80.
 2. ALL WELDS TO BE FILLET TYPE, GROUND SMOOTH.
 3. ALL HOLES TO BE SHOP DRILLED.
 4. ALL EXPOSED METAL TO BE PRIMED AND PAINTED. FINAL COAT TO BE ENAMEL, COLOR SAFETY YELLOW.
 5. 2 APPROPRIATE PADLOCKS KEYED ALIKE TO BE PROVIDED BY CONTRACTOR.
 6. ALL CONCRETE FOR FOOTING SHALL BE FIELD Poured WITH PLANT MIXED CONCRETE.
 7. 2 OUTSIDE BOLLARDS TO BE FILLED WITH CONCRETE AND TO HAVE NO HOLES DRILLED

PLOT SCALE:
PLOT NAME:

REV. DATE:

ORIGINATOR: CITY OF MADISON, STREETS DIVISION



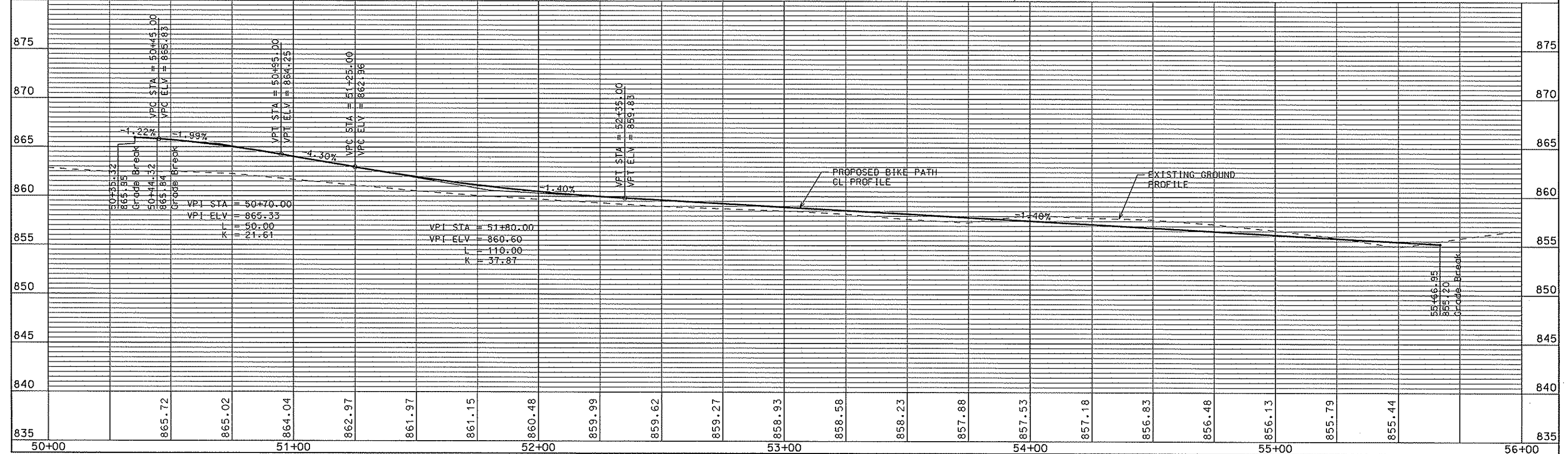
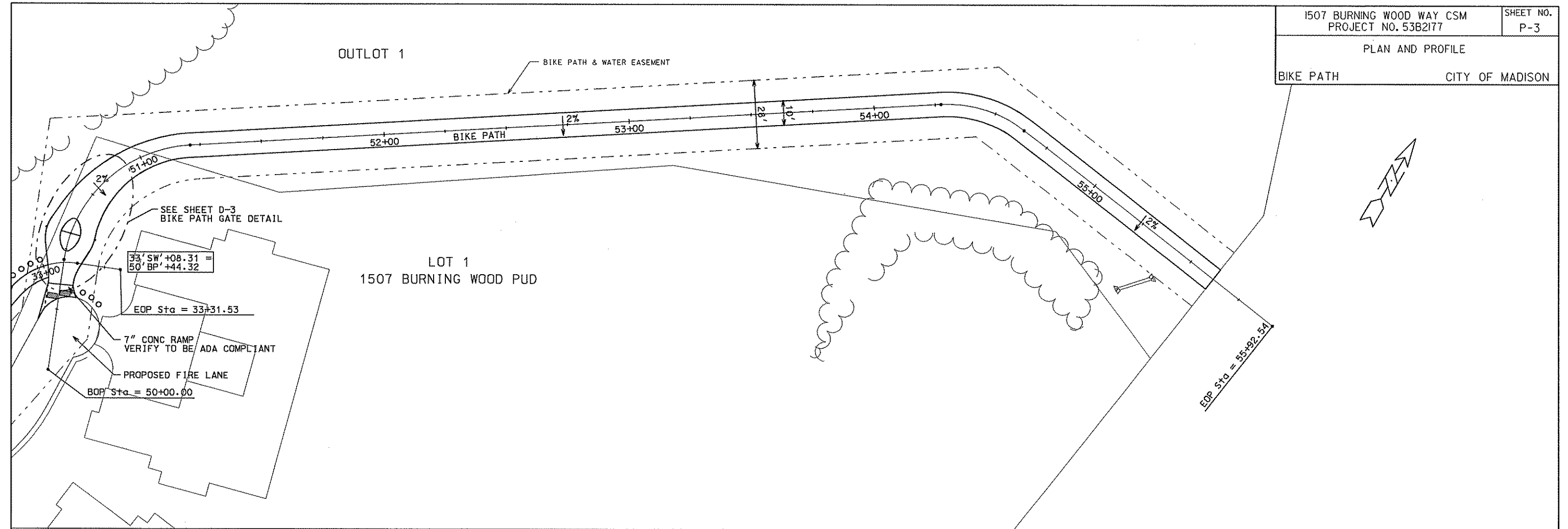
PLOT SCALE: _____
 PLOT NAME: _____
 REV. DATE: _____
 ORIGINATOR: CITY OF MADISON, STREET DIVISION

PLAN AND PROFILE

BIKE PATH CITY OF MADISON

OUTLOT 1

LOT 1
1507 BURNING WOOD PUD



PLOT SCALE:
PLOT NAME:
REV. DATE:

ORIGINATOR: CITY OF MADISON - STREETS DIVISION

STORM SEWER SCHEDULE

ALIGNMENT CODES:

"C" = COMANCHE WAY

1507 BURNING WOOD WAY
PROJECT NO. 53B2177

SHEET NO.
U-3

STORM SEWER SCHEDULE CITY OF MADISON

STRUCTURES

STRUC. NO.	STATION	LOCATION (OFFSET)	TYPE	TOP OF CASTING	E.I.	DEPTH	NOTES
S-0	10'C+40.75	LT-30.00	CONC COLLAR	---	857.00	---	
** S-1	10'C+40.75	LT-19.44	TYP 1 TERRACE INL	861.54	852.50	9.04	FP; SEE SDD 5.7.12; (1)
S-2	10'C+91.70	LT-35.25	3X3 SAS	863.19	858.60	4.59	LP; W/R-3067-7400
S-3	11'C+07.29	LT-15.80	H INLET	862.35	858.76	3.59	LP; W/SALVAGED CASTING
S-4	11'C+05.00	RT-15.50	EX INLET	862.06	858.92	3.14	LP; W/R-3067-7400; (2)
S-5	10'C+79.90	LT-60.12	H INLET	863.27	859.86	3.41	LP; W/R-3067-7400

PIPES

PIPE NO.	FROM (DNSTM)	TO (UPSTM)	LGTH (FT)	DISCH. E.I.	INLET E.I.	SLOPE (%)	PIPE SIZE	TYPE	NOTES
P-1	S-0	S-1	10	857.00	857.50	5.00%	24"	RCP	NOTE 1
P-2	S-1	S-2	106	858.00	858.60	0.57%	18"	RCP	
P-3	S-2	S-3	31	858.60	858.76	0.52%	12"	RCP	NCM
P-5	S-2	S-5	34	859.10	859.86	2.24%	12"	RCP	

** REVISED 5-7-08 CCS

PLOT SCALE: ---

PLOT NAME: ---

REV. DATE: ---

STANDARD NOTES:

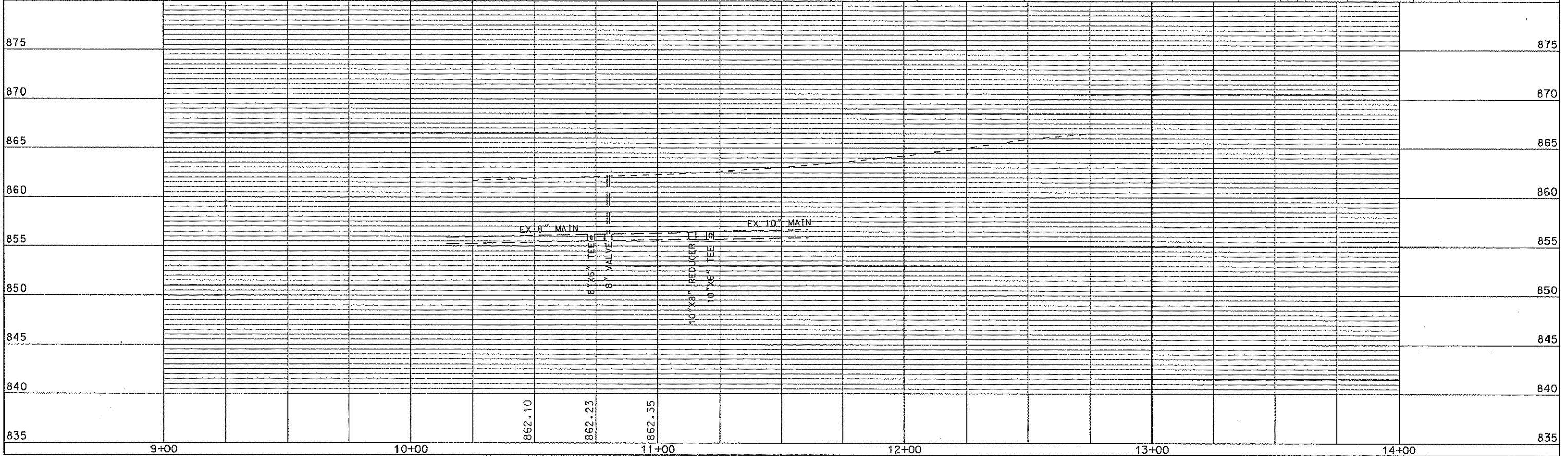
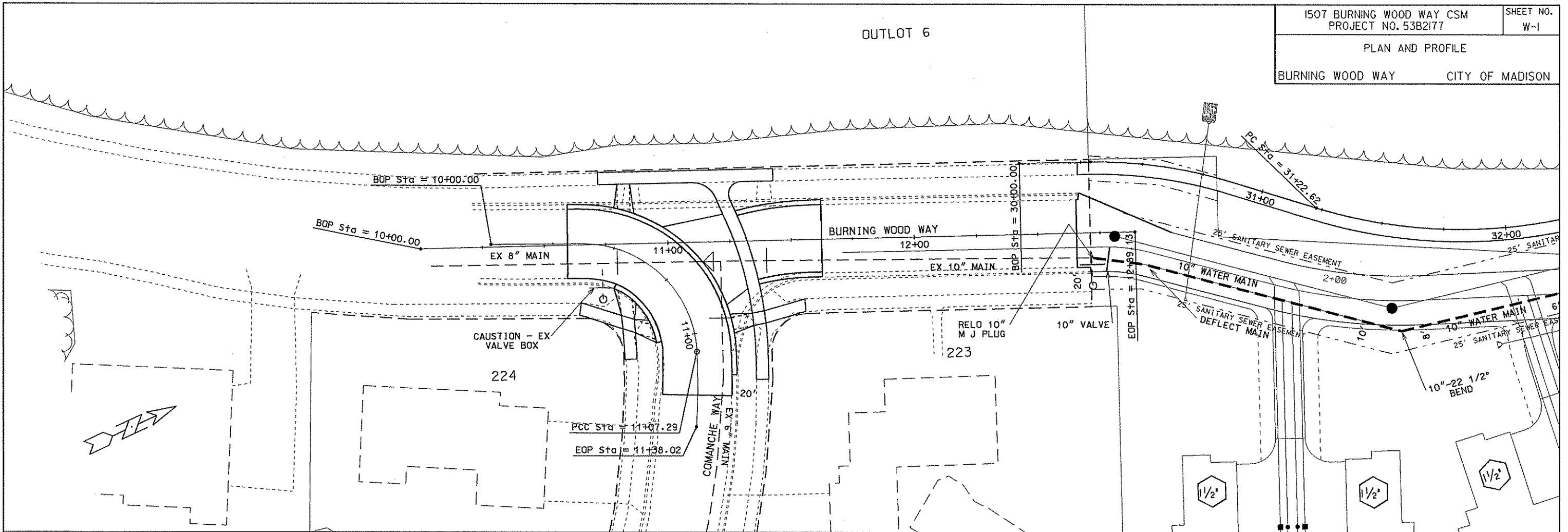
- ABBREVIATIONS: AE = APRON ENDWALL; RCP = REINFORCED CONCRETE PIPE; HERCP = HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE; DNA = DOES NOT APPLY; SAS = SEWER ACCESS STRUCTURE; LP = LOW POINT INLET STRUCTURE; FP = FIELD POURED STRUCTURE; TR = TOP OF CONCRETE ROOF; NCM = NO CROWN MATCH FOR PIPES
- APPROXIMATE DISCHARGE E.I. GIVEN, ADJUST E.I. AND PIPE SLOPE IN THE FIELD.
- TOP OF CASTING GRADE GIVEN IS THE TOP OF CURB FOR INLET STRUCTURES AND THE FLOWLINE OF THE CLOSED CASTING FOR SAS's.
- TOP OF CONCRETE ROOF (TR) IS 1.25' BELOW TOP OF CASTING UNLESS OTHERWISE NOTED.
- ALL REINFORCED CONCRETE PIPES TO BE CLASS III UNLESS OTHERWISE NOTED.
- SURVEYOR TO CONFIRM THAT ALL INLET STATION / OFFSETS LINE UP WITH PROPOSED CURB AND GUTTER.
- ALL STRUCTURES CALLED OUT AS FIELD POURED SHALL BE FIELD POURED. ALL OTHER STRUCTURES (NOT INDICATED AS FIELD POURED) SHALL BE SUBMITTED TO CITY ENGINEERING FOR APPROVAL IF PRECAST STRUCTURES ARE PREFERRED. CONTACT COREY STELLJES OF CITY ENGINEERING AT (608) 266-9721 FOR PRECAST APPROVALS, OR FAX SHOP DRAWINGS TO (608)264-9275.

SPECIFIC NOTES

- (1) 24" STUB FOR FUTURE PROJECT. CONNECT TO EX 15" STORM WITH COLLAR. RECONNECT PIPE TO SOUTH/EAST
- (2) PLUG PIPE IN FRONT OF INLET; ADJUST

OUTLOT 6

1507 BURNING WOOD WAY CSM
PROJECT NO. 53B2177
SHEET NO. W-1
PLAN AND PROFILE
BURNING WOOD WAY CITY OF MADISON

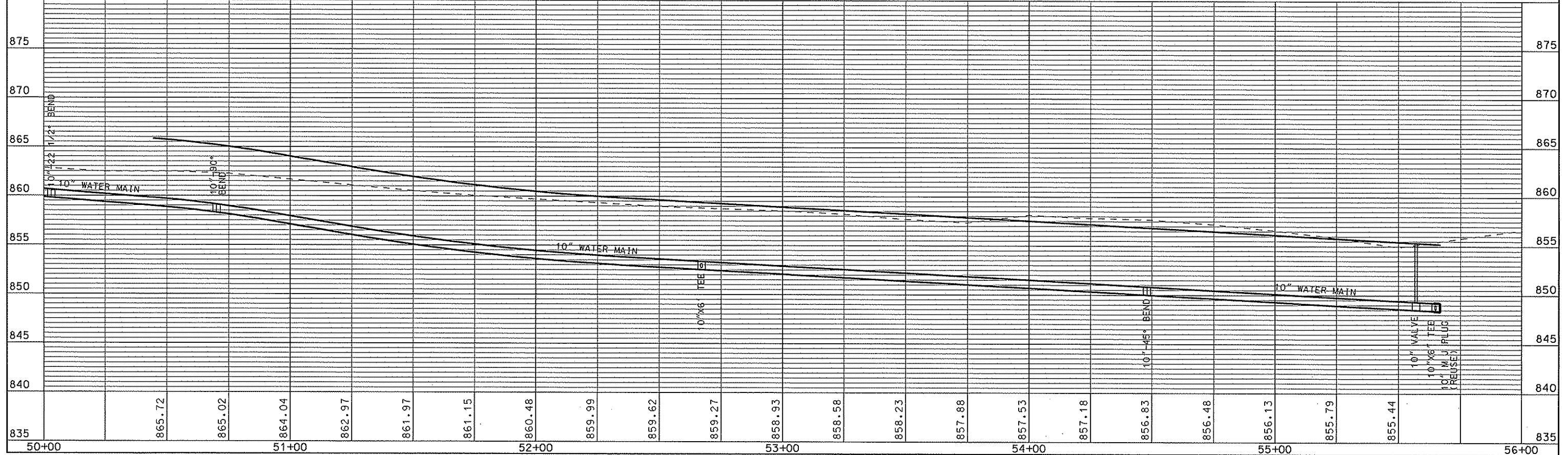
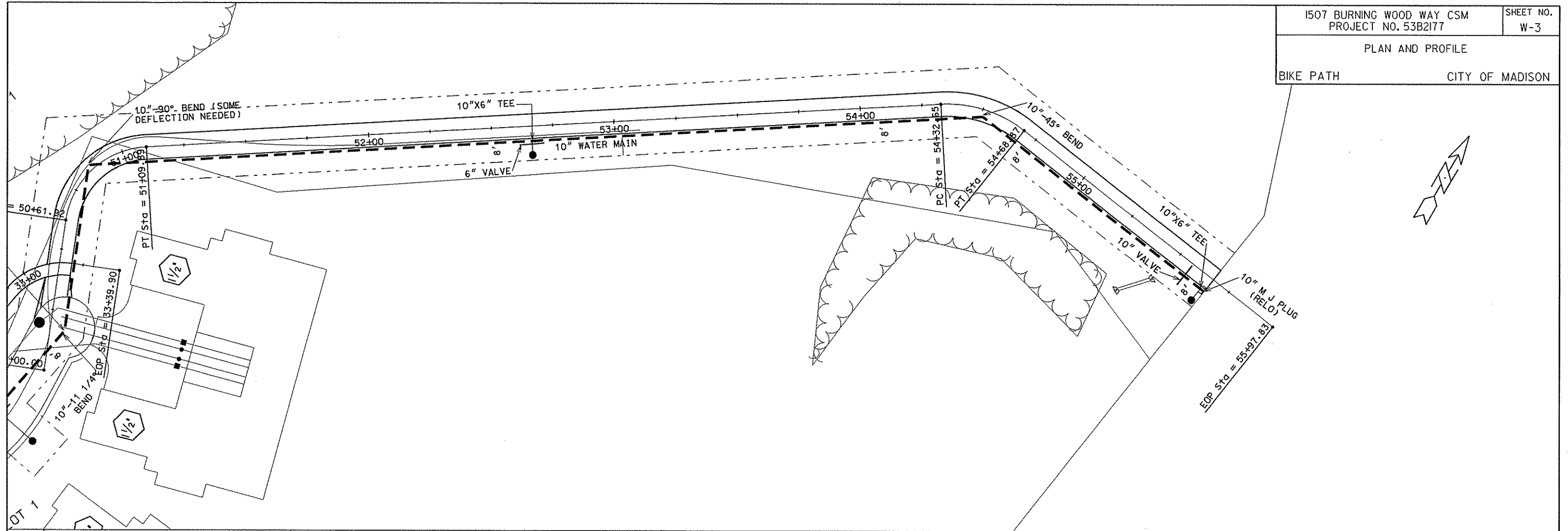


ORIGINATOR: CITY OF MADISON, STREETS DIVISION

REV. DATE: PLOT NAME: PLOT SCALE:

PLAN AND PROFILE

BIKE PATH CITY OF MADISON



ORIGINATOR: CITY OF MADISON, STREETS DIVISION

PLOT SCALE: PLOT NAME: REV. DATE: